FINAL PUD PLAN

PART OF THE NORTHWEST QUARTER OF SECTION 17, TOWNSHIP 1 SOUTH, RANGE 66 WEST OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF ADAMS, STATE OF COLORADO

LEGAL DESCRIPTION

A PARCEL OF LAND BEING TRACTS A THRU K, O, Q, TRACTS L THRU N AND P BLOCK 1, TRACTS L THRU N AND P BLOCK 2, TRACTS L AND P BLOCK 3, TRACTS P BLOCK 4, ALICIA DRIVE AND SOUTH 15TH AVENUE, BROMLEY FARMS, A SUBDIVISION PLAT RECORDED UNDER RECEPTION NO. 2006000987859 IN THE RECORDS OF THE ADAMS COUNTY, COLORADO, CLERK AND RECORDER'S OFFICE, LOCATED IN THE NORTHWEST QUARTER OF SECTION 17, TOWNSHIP 1 SOUTH, RANGE 66 WEST OF THE SIXTH PRINCIPAL MERIDIAN, SAID COUNTY AND STATE, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTH QUARTER CORNER OF SAID SECTION 17

THENCE ALONG THE EASTERLY LINE OF SAID NORTHWEST QUARTER SOUTH 00°19'47" EAST, A DISTANCE OF 85.00 FEET TO THE NORTHEAST CORNER OF SAID TRACT G AND THE POINT OF BEGINNING.

THENCE CONTINUING ALONG SAID EASTERLY LINE SOUTH 00°19'47" EAST, A DISTANCE OF 1,438.25 FEET TO THE WESTERLY RIGHT-OF-WAY LINE OF SOUTH 19TH AVENUE AND THE BEGINNING OF A NON-TANGENT CURVE CONCAVE EASTERLY HAVING A RADIUS OF 378.50 FEET, THE RADIUS POINT OF SAID CURVE BEARS SOUTH 68°09'11" EAST;

THENCE ALONG SAID WESTERLY RIGHT-OF-WAY LINE THE FOLLOWING (2) COURSES:

- 1) SOUTHERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 22°10'36" AN ARC LENGTH OF 146.50 FEET;
- 2) TANGENT TO SAID CURVE SOUTH 00°19'47" EAST, A DISTANCE OF 913.01 FEET TO THE NORTHERLY RIGHT OF WAY LINE OF INDIAN PAINTBRUSH DRIVE AND THE BEGINNING OF A TANGENT CURVE CONCAVE NORTHWESTERLY HAVING A RADIUS OF 29.00 FEET;

THENCE ALONG SAID NORTHERLY RIGHT-OF-WAY LINE THE FOLLOWING (3) COURSES:

- 1) SOUTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 89°47'02" AN ARC LENGTH OF 45.44 FEET;
- 2) TANGENT TO SAID CURVE SOUTH 89°27'15" WEST, A DISTANCE OF 2,548.89 FEET TO THE BEGINNING OF A TANGENT CURVE CONCAVE NORTHEASTERLY HAVING A RADIUS OF 21.00 FEET;
- 3) NORTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 90°23'11" AN ARC LENGTH OF 33.13 FEET TO THE EASTERLY RIGHT-OF-WAY LINE OF CHAMBERS ROAD;

THENCE ALONG SAID EASTERLY RIGHT-OF-LINE NORTH 00°09'34" WEST, A DISTANCE OF 2,459.71 FEET TO THE SOUTHERLY RIGHT-OF-WAY LINE OF BROMLEY LANE AND THE BEGINNING OF A TANGENT CURVE CONCAVE SOUTHEASTERLY HAVING A RADIUS OF 40.43 FEET.

THENCE ALONG SAID SOUTHERLY RIGHT-OF-WAY LINE THE FOLLOWING (2) COURSES:

- 1) NORTHEASTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 89°34'15" AN ARC LENGTH OF 63.20 FEET;
- 2) TANGENT TO SAID CURVE NORTH 89°24'41" EAST, A DISTANCE OF 2,579.31 FEET TO THE POINT OF BEGINNING

EXCLUDING THEREFROM TRACTS B, G & J, BROMLEY FARMS, A SUBDIVISION PLAT RECORD UNDER RECEPTION NO. 2006000987859 IN THE RECORDS OF THE ADAMS COUNTY, COLORADO, CLERK AND RECORDER'S OFFICE.

CONTAINING AN AREA OF 135.14 ACRES (5.886,712 SQUARE FEET) MORE OR LESS.

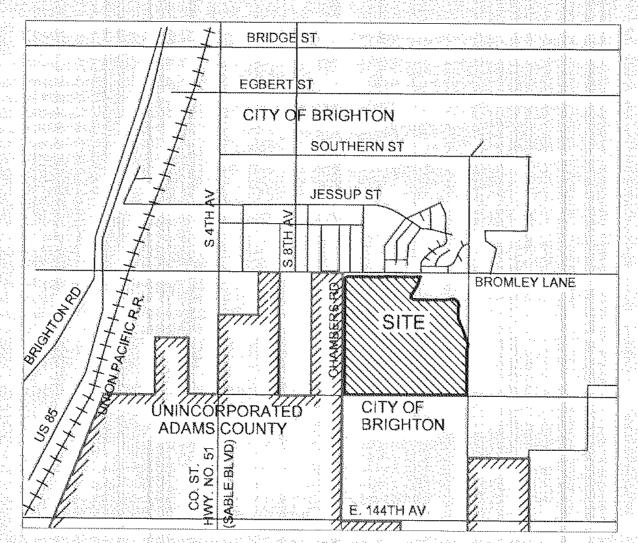
NOTES

BASIS OF BEARINGS

BEARINGS ARE GRID BEARINGS COLORADO COORDINATE SYSTEM NAD 83 (2007)
BASED UPON THE EASTERLY LINE OF THE NORTHWEST QUARTER OF SECTION
17, TOWNSHIP 1 SOUTH, RANGE 66 WEST, OF THE 6TH PRINCIPAL MERIDIAN,

17, TOWNSHIP 1 SOUTH, RANGE 66 WEST, OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF ADAMS, STATE OF COLORADO AS BEARING S00°19'47"E BETWEEN THE MONUMENTS SHOWN HEREON.

- 2. APPROVAL OF THIS DOCUMENT MAY CREATE A VESTED PROPERTY RIGHT PURSUANT TO ARTICLE 68 OF TITLE 24, COLORADO REVISED STATUTES, AS AMENDED
- 3. THE BROMLEY FARMS PROPERTY MUST BE RE-SUBDIVIDED OR RE-PLATTED IN ORDER TO MATCH THE LAYOUT OF THE BROMLEY FARMS PUD, SUCH AS THE LOCATION OF RIGHT-OF-WAY, PARKS AND OPEN SPACES, PLANNING AREA BOUNDARIES, ETC., PRIOR TO ANY DEVELOMENT ON THE PROPERTY OR THE SUBMITTAL OF ANY BUILDING PERMITS.



VICINITY MAP SCALE: 1" = 2000"

NOTES CONTINUED

4. A FINAL ALIGNMENT FOR THE ROADWAY THAT PROVIDES ACCESS TO THE WATER PARK, GENERALLY LOCATED BETWEEN SOUTH 15TH AVENUE AND THE BRIGHTON OASIS WATER PARK SHALL BE CORDINATED WITH THE BRIGHTON PARKS AND RECREATION DEPARTMENT AND DEDICATED AS A RIGHT-OF WAY, AT THE TIME OF FINAL PLAT AND/OR RE-PLAT.

CITY APPROVAL

ACCEPTED BY THE CITY COUNCIL OF THE CITY OF BRIGHTON THIS DAY
OF 12011.

MAYOR
ATTEST, CITY CLERK (SEAL)

COUNTY CLERK AND RECORDER'S CERTIFICATE

P.U.D. NO.

RECEPTION NO.

ACCEPTED FOR FILING IN THE OFFICE OF THE COUNTY CLERK AND RECORDER OF ADAMS COUNTY AT BRIGHTON, COLORADO ON THIS DAY OF JAMES V. 20/2.

AT 1:20:00 O'CLOCK M.

MAPWAI LO AND RECORDER

201200000 7/28

BY: DEPUTY CLERK

(SEAL)

CERTIFICATE OF OWNERSHIP

WE THE UNDERSIGNED, BEING THE OWNER(S) AND/OR DEVELOPER(S) OF THE LAND HEREIN DESCRIBED LOCATED IN THE CITY OF BRIGHTON, COUNTY OF ADAMS, STATE OF COLORADO, HEREBY SUBMIT THIS PLAN AND AGREE TO PERFORM UNDER THE TERMS NOTED HEREON.

OWNER:
BY:

BY:

FOR:

THOMAS L. HARTINY

DAT

NOTARIAL

THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME
THIS | O DAY OF DAY

SCOTT DAVID BENNETTS
NOTARY PUBLIC
STATE OF COLORADO



SHEET INDEX

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- GENERAL NOTES
 PUD SITE PLAN
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- 14 CONCEPTUAL DRAINAGE PLAN

ENGINEER

REDLAND 8000 SOUTH LINCOLN STREET, SUITE 206 LITTLETON, CO. 80122 (720) 283-6783

Engineering Great Places
B000 South Tracks Street, 1200 (Misser, CO 2012)
ONOT 1722-183 6183 (www.redusnd.com

LAND PLANNER

KEPHART
2555 WALNUT STREET
DENVER, CO. 80205
(303) 832-4474
CONTACT: MR. CHRIS GRADY

CONTACT: MR. FRED TAFOYA



LANDSCAPE ARCHITECT

BRITINA DESIGN GROUP 7600 GRANDVIEW AVENUE, #210 ARVADA, CO. 80002 (303) 456-2887 CONTACT: MR. BOB COURI





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FINAL PUD PLAN

PART OF THE NORTHWEST QUARTER OF SECTION 17, TOWNSHIP 1 SOUTH, RANGE 66 WEST OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF ADAMS, STATE OF COLORADO

STATEMENT OF PURPOSE AND INTENT

THE BROMLEY FARMS PUD IS A 135.14 ACRE COMMUNITY IN THE CITY OF BRIGHTON, COLORADO, IT IS ENVISIONED AS A DISTINCTIVE RESIDENTIAL COMMUNITY INTEGRATING A VARIETY OF INTIMATELY SCALED RESIDENTIAL NEIGHBORHOODS CONNECTED BY A COMPREHENSIVE OPEN SPACE NETWORK, WITH A VARIETY OF RECREATIONAL OPPORTUNITIES AND SCENIC AMENITIES.

THE FOLLOWING LIST OUTLINES AREAS IN WHICH THIS COMMUNITY INCORPORATES DESIGN ELEMENTS THAT CONTRIBUTE TO EXCEPTIONAL HIGH QUALITY DESIGN:

- 1. CREATE SMALLER RESIDENTIAL PARCELS WITH A VARIETY OF RESIDENTIAL PRODUCTS AND DENSITIES CREATING A RICH COMMUNITY TEXTURE AND DIVERSITY.
- 2. CREATE A MIX OF RESIDENTIAL PRODUCT OPTIONS WITHIN EACH PLANNING AREA TO ENSURE DIVERSITY AND BROADEN MARKET OFFERINGS.
- INCORPORATE A DETACHED SIDEWALK WITH A TREE LAWN AND STREET TREES SPACED AN AVERAGE OF 40 FEET ON CENTER CREATING A TIMELESS NEIGHBORHOOD WITH APPROPRIATE SCALE AND CHARACTER FOR ALL PUBLIC STREETS.
- 4. PROVIDE A COMPREHENSIVE PEDESTRIAN NETWORK THROUGHOUT THE COMMUNITY CONNECTING TO OFFSITE REGIONAL PEDESTRIAN CORRIDORS, AND INCLUDING THE FOLLOWING:
 - A. 10 FOOT CONCRETE SIDEWALK ALONG THE SOUTH SIDE OF BROMLEY LANE, INCLUDING ALONG THE HISTORIC FARM AND OASIS AQUATIC CENTER FRONTAGE. 10 FOOT CONCRETE SIDEWALK ALONG THE WEST SIDE OF 19TH AVENUE.
 - B. 8 FOOT CONCRETE SIDEWALK ON THE EAST SIDE OF CHAMBERS
 ROAD
 - G. 10 FOOT CONCRETE SIDEWALK ALONG THE FULTON DITCH WILL INTERCONNECT TO THE REGIONAL TRAIL SYSTEM.
 - D. 6 FOOT AND 8 FOOT CONCRETE SIDEWALKS ALONG INTERNAL COLLECTOR STREETS.
 - E. 4 FOOT CONCRETE SIDEWALKS ALONG LOCAL STREETS AND PASEOS.
 - F. STREET TREE LAWNS SHALL BE CONSISTENT WITH TYPICAL ROAD SECTIONS WITHIN THIS PUD AND SHALL BE A MINIMUM OF 6 FEET IN WIDTH.
- NCORPORATE SIGNIFICANT PERIMETER LANDSCAPE BUFFERS AS FOLLOWS;
 - A. NORTH BOUNDARY
- 40 FT.
- B. EAST AND WEST BOUNDARIES 30 FT
- C. SOUTH BOUNDARY SUBSTANTIAL BUFFER CONSISTING OF THE FULTON DITCH, THE REGIONAL OUTFALL, NEIGHBORHOOD PARKS, AND OPEN SPACE.
- PROVIDE A VARIETY OF PASSIVE AND ACTIVE RECREATIONAL OPPORTUNITIES THROUGHOUT THE COMMUNITY.

AUTHORITY AND DEFINITIONS

AUTHORITY

THE CITY OF BRIGHTON'S RESIDENTIAL DESIGN STANDARDS TOGETHER WITH THE BROMLEY FARMS RESIDENTIAL DESIGN STANDARDS (BFRDS), SHALL APPLY TO ALL PROPERTY CONTAINED WITHIN THE BROMLEY FARMS PLANNED UNIT DEVELOPMENT.

THESE GUIDELINES ARE THE GOVERNING STANDARDS FOR REVIEW, APPROVAL AND MODIFICATION OF DEVELOPMENT ACTIVITIES OCCURRING ON THE PROPERTY. THE ORDINANCES, RULES, REGULATIONS AND REQUIREMENTS OF THE CITY SHALL APPLY WHERE THE PROVISIONS OF THESE GUIDELINES DO NOT SPECIFICALLY ADDRESS A PARTICULAR SUBJECT.

- 2. DEFINITIONS
 - A. ACCESSORY BUILDING:

A DETACHED SUBORDINATE BUILDING LOCATED ON THE SAME LOT (OR A CONTINUOUS LOT IN THE SAME OWNERSHIP) WITH THE PRINCIPAL BUILDING, USED FOR ACCESSORY USE. IF AN ACCESSORY BUILDING IS ATTACHED TO THE PRINCIPAL BUILDING BY A COMMON WALL, SUCH ACCESSORY BUILDING SHALL BE CONSIDERED PART OF THE MAIN BUILDING. FOR THE PURPOSES OF THIS SECTION, A COMMON WALL IS DEFINED AS A MINIMUM FOUR FOOT (4') SECTION OF WALL THAT SEPARATES AND/OR CONNECTS TWO (2) ADJACENT ROOMS. A SHED, COVERED PATIO, GREENHOUSE, OR STORAGE BUILDING, ETC. IS INCLUDED AS AN ACCESSORY BUILDING. REAR AND SIDE SETBACKS FOR ACCESSORY BUILDINGS SHALL BE 3 FEET AND 5 FEET, RESPECTIVELY. THE MAXIMUM BUILDING HEIGHT FOR AN ACCESSORY BUILDING SHALL BE 16 FEET, EXCEPT AS PROVIDED HEREIN. THE MAXIMUM BUILDING SIZE OF AN ACCESSORY BUILDING SHALL BE 400 SQUARE FEET.

B. DETACHED GARAGE:

AN ACCESSORY BUILDING DESIGNED FOR THE SHELTER OR STORAGE OF MOTOR VEHICLES OR TRAILERS, OWNED OR OPERATED BY OCCUPANTS OF THE PRINCIPLE BUILDING. THE MAXIMUM BUILDING HEIGHT FOR A DETACHED GARAGE SHALL BE 25 FEET, THE MAXIMUM BUILDING SIZE OF A DETACHED GARAGE SHALL BE 900 SQUARE FEET FOR A SINGLE FAMILY LOT AND 600 SQUARE FEET FOE A DUPLEX LOT.

C. CARRIAGE UNIT:

AN ACCESSORY BUILDING CONSISTING OF LIVING QUARTERS ABOVE A DETACHED GARAGE THAT MAY BE USED AS WORKSHOP, HOME OFFICE STUDIO OR LIVE-IN QUARTERS; PROVIDED HOWEVER, IN NO EVENT SHALL A CARRIAGE UNIT EXCEED 900 SQUARE FEET, THE MAXIMUM BUILDING HEIGHT FOR A CARRIAGE UNIT SHALL BE 30 FEET, CARRIAGE UNITS ARE PERMITTED ON SINGLE-FAMILY DETACHED RESIDENTIAL LOTS ONLY. RENTAL OR SALE OF CARRIAGE UNITS IS PROHIBITED EXCEPT WITH THE RENTAL TO THE SAME LESSEE OR SALE TO THE SAME OWNER, OF THE PRIMARY BUILDING. RESIDENTIAL PROPERTY SHALL BE RESTRICTED BY DEED AND PRIVATE COVENANT TO PROVIDE FOR THE FOREGOING PROHIBITION. EACH PLAT SHALL BE LIMITED TO A MAXIMUM 30% OF SINGLE FAMILY DETACHED LOTS THAT MAY HAVE A CARRIAGE UNIT. THERE SHALL BE NO ABILITY TO TRANSFER CARRIAGE UNITS BETWEEN PLATS, EACH PLAT SHALL IDENTIFY WHICH SINGLE FAMILY DETACHED LOTS MAY HAVE CARRIAGE UNITS. CARRIAGE UNIT LOCATIONS WITHIN EACH PLAT MAY BE MODIFIED (UPON PETITION BY THE APPLICANT) AT THE SOLE DISCRETION OF THE COMMUNITY DEVELOPMENT DIRECTOR AND AS LONG AS NO MORE THAN 30% OF THE LOTS WITHIN EACH PLAT SHALL HAVE CARRIAGE UNITS.

- AT LEAST ONE ADDITIONAL PAVED PARKING SPACE SHALL BE
 PROVIDED FOR EACH CARRIAGE UNIT.
- D. BUILDING HEIGHT:

THE VERTICAL DISTANCE FROM THE ESTABLISHED DATUM LINE AND THE REFERENCE POINT OF THE ROOF. THE DATUM LINE IS DETERMINED AT THE LOWEST MEASUREMENT OF EITHER THE POINT WHERE THE GRADE IS AT ITS HIGHEST WHEN IN CONTACT WITH THE BUILDING OR THE POINT MEASURED TEN FEET (10') VERTICALLY, FROM A SPOT FIVE FEET (5') AWAY FROM THE LOWEST ELEVATION OF GRADE IN CONTACT WITH THE BUILDING. THE REFERENCE POINT OF THE ROOF IS EITHER THE HIGHEST POINT OF THE COPING OF A FLAT ROOF, THE DECK LINE OF A MANSARD ROOF, OR THE MEAN HEIGHT LEVEL BETWEEN EAVES AND HIGHEST RIDGE FOR A GABLE OR HIP ROOF. CHIMNEYS, SPIRES, TOWERS, ELEVATORS, PENTHOUSE, TANKS, AND SIMILAR PROJECTIONS SHALL NOT BE INCLUDED IN CALCULATING THE HEIGHT UNLESS SPECIFICALLY REFERRED TO.

- THE MAXIMUM BUILDING HEIGHT FOR A SINGLE FAMILY DETACHED
 HOME SHALL BE THIRTY-FIVE FEET (35').
- THE MAXIMUM BUILDING HEIGHT FOR SINGLE FAMILY ATTACHED HOME SHALL BE THIRTY-FIVE FEET (35').
- 6. THE MAXIMUM BUILDING HEIGHT FOR MULTI-FAMILY SHALL BE FORTY-FIVE FEET (45').
- FOR PLANNING AREA 2, THE MAXIMUM BUILDING HEIGHT FOR GROUP FACILITIES AND MULTI-FAMILY SHALL BE THIRTY FIVE FEET (35').
- CLUSTER DEVELOPMENT:

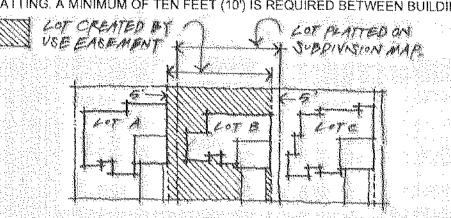
A FORM OF PLANNED DEVELOPMENT WHEREBY RESIDENTIAL UNITS ARE GROUPED TOGETHER TO PROVIDE AN IMPROVED DESIGN, MORE EFFICIENT INFRASTRUCTURE, MORE COMMUNITY GREEN OR OPEN SPACE, SHARED PARKING, ACCESS AND OTHER AMENITIES THAT MIGHT NOT BE OBTAINABLE THROUGH CONVENTIONAL DEVELOPMENT.

- F. COMMON AREA ACCESSORY STRUCTURES;
 STRUCTURES THAT MIGHT INCLUDE PICNIC SHELTERS IN OPEN SPACES,
 COMMUNITY STRUCTURES, OR SIMILAR USES. ALL OTHER COMMON AREA
 ACCESSORY STRUCTURES MUST COMPLY WITH EXISTING CITY CODES. THE
 MAXIMUM STRUCTURE HEIGHT FOR COMMON AREA ACCESSORY
 STRUCTURES SHALL BE 25 FEET. THE MAXIMUM BUILDING SIZE FOR
 COMMON AREA ACCESSORY STRUCTURES SHALL BE 1000 SQUARE FEET.
- G. GROUP CARE FACILITY:
 A FACILITY INCLUDING ASSISTED LIVING, NURSING HOME, SENIOR
 HOUSING, IINDEPENDENT LIVING, CONGREGATE CARE AND/OR MEMORY
 CARE FACILITIES:
- H: HOMEOWNER'S ASSOCIATION (HOA):
 A DULY INCORPORATED ORGANIZATION COMPRISED OF HOMEOWNERS IN A
 PARTICULAR COMMUNITY FOR THE PURPOSE OF ENFORCING
 RESTRICTIONS AND MANAGING ALL OR A PORTION OF BROMLEY FARMS.
- LOT WIDTH:
 THE HORIZONTAL DISTANCE BETWEEN SIDE LOT LINES MEASURED AT THE
 ESTABLISHED FRONT YARD SETBACK LINE.

- J. MULTI-FAMILY DWELLING:
 3-PLEX THROUGH 36- PLEX ATTACHED APARTMENTS, AND CONDOMINIUMS.
- K. PARCEL GROSS BOUNDARY:
 PARCEL GROSS BOUNDARIES ARE FIXED BY THE PUD PLAN. PARCEL
 BOUNDARIES SHOWN ARE TO THE CENTERLINE OF LOCAL STREETS OR
 BETWEEN LAND USES—AND TO THE EDGE OF R.O.W. FOR INTERNAL
 COLLECTORS. DENSITY SHALL BE COMPUTED BASED UPON THE PARCEL
 GROSS BOUNDARY INCLUDING STREETS—AND OPEN SPACE.
- L. PATIO HOME:

A SINGLE FAMILY DWELLING UNIT SITUATED ON ONE LOT, SET TO ONE SIDE NEAR OR ON THE SIDE PROPERTY LINE. GENERALLY EACH LOT INCORPORATES A USE EASEMENT ON THE ADJACENT LOT CREATING A LARGER SIDE AND/OR REAR CORNER PATIO AND/OR YARD SPACE, THEREBY INCREASING THE OUTDOOR LIVING AREA.

- M. SINGLE FAMILY ATTACHED DWELLING;
 A DWELLING WITH PRIMARY GROUND FLOOR ACCESS TO THE OUTSIDE,
 WHICH IS ATTACHED TO ANOTHER UNIT BY A PARTY WALL WITHOUT
 OPENINGS. SINGLE FAMILY ATTACHED DWELLINGS INCLUDE PAIRED
 HOMES AND UP TO EIGHT-UNIT TOWNHOME BUILDINGS.
- N. SINGLE FAMILY DETACHED DWELLING:
 SINGLE FAMILY DETACHED DWELLING IS ONE DWELLING UNIT BUT MAY
 INCLUDE A DETACHED CARRIAGE UNIT. SINGLE FAMILY DETACHED
 DWELLINGS MAY BE CONFIGURED IN A CLUSTER AND UTILIZE PATIO HOMES
 OR ALLEY-LOADED PRODUCTS.
- O. USE EASEMENTS / USEABLE SIDE YARDS / PATIO HOMES:
 A USEABLE SIDE YARD MAY BE CREATED BY ESTABLISHING A USE
 EASEMENT ON AN ADJACENT LOT AND ALLOWING FULL USE OF THE SIDE
 YARD EXTENDING TO THE FOUNDATION OR WALL OF THE ADJACENT
 STRUCTURE, FOR FIRE AND BUILDING CODE PURPOSES, THE STRUCTURE IS
 PLACED ADJACENT TO THE SIDE LOT LINE, AND NOT DIRECTLY ON THE LOT
 LINE, FIVE FOOT (5') USE AND MAINTENANCE EASEMENTS SHALL BE
 REQUIRED FOR MAINTENANCE, DRAINAGE AND ROOF OVERHANGS TO THE
 ADJACENT PROPERTY. EASEMENTS FOR PRIVATE MAINTENANCE AND
 DRAINAGE SHALL BE PLACED ON A CONTINUOUS BLOCK AT THE TIME OF
 PLATTING, A MINIMUM OF TEN FEET (10') IS REQUIRED BETWEEN BUILDINGS



USE EASEMENTS ALLIN FOR USEABLE SIDE TARDS,
- S FOOT USE EASEMENT GRANTED FOR USE
BY LOT B FROM LOT A.

GENERAL LOCATION AND LAND USE

- PROJECT LOCATION AND SURROUNDING LAND USE:
 THE PROPOSED COMMUNITY IS LOCATED SOUTH OF BROMLEY LANE, AND IS
 BOUNDED ON THE EAST BY SOUTH 19TH AVENUE AND ON THE WEST BY
 CHAMBERS ROAD. THE PROPERTY IS SURROUNDED BY EXISTING RESIDENTIAL
 DEVELOPMENT TO THE NORTH AND SOUTH, PROPOSED RESIDENTIAL TO THE
 EAST, AND AGRICULTURAL TO THE WEST.
- PROJECT ACCESS:
 THE PRIMARY MEANS OF ACCESS TO BROMLEY FARMS INCLUDES BROMLEY
 LANE TO THE NORTH, CHAMBERS RD. TO THE WEST, AND SOUTH 19TH AVENUE
 TO THE EAST:

UNDERLYING ZONE DISTRICTS

THE UNDERLYING ZONE DISTRICT FOR EACH PLANNING AREA IS AS FOLLOWS:

PA-1: R-3 PA-2: R-3 PA-3: R-1-A PA-4: R-1 PA-5: R-1-A PA-6: R-1



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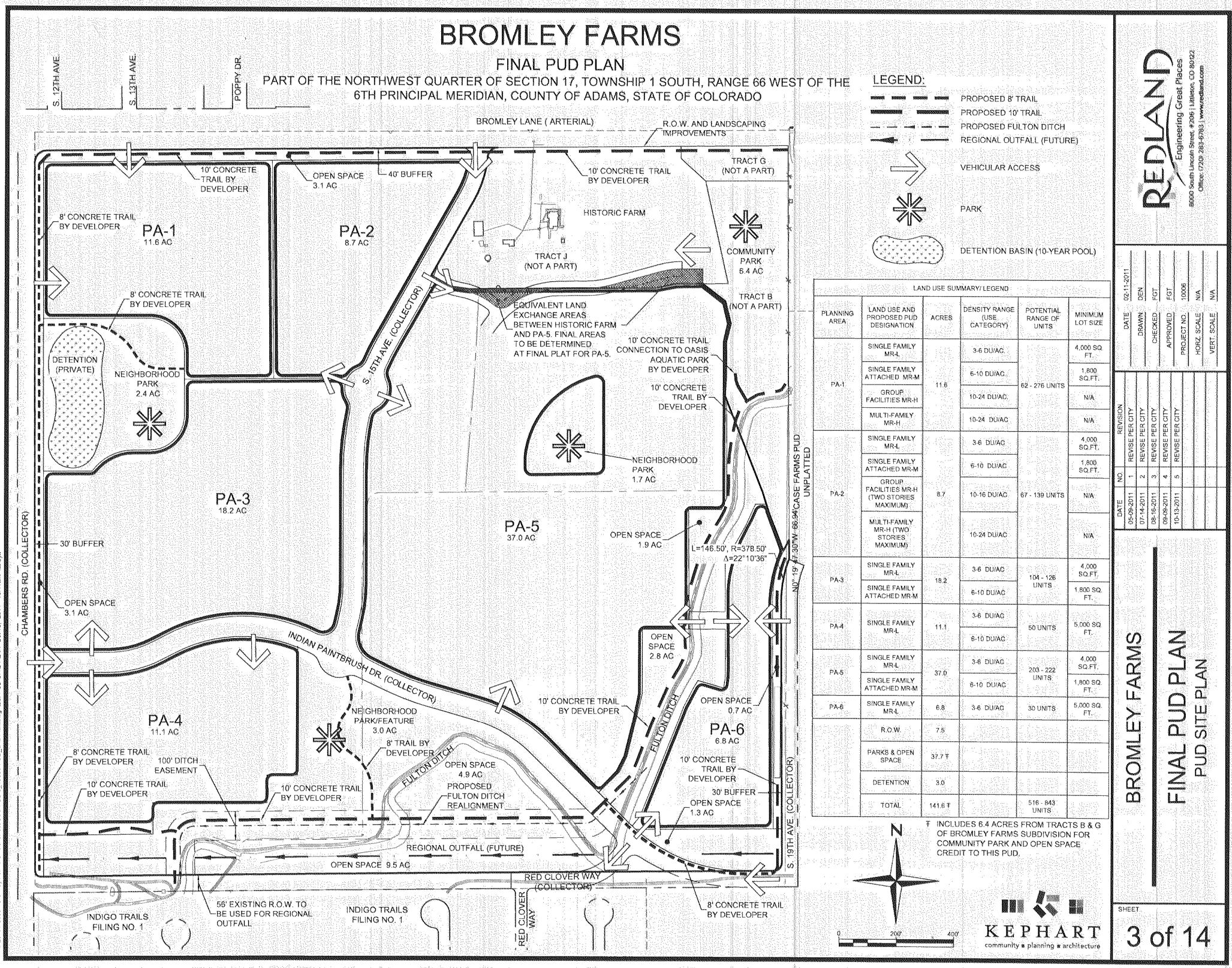
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FINAL PUD PLAN

PART OF THE NORTHWEST QUARTER OF SECTION 17, TOWNSHIP 1 SOUTH, RANGE 66 WEST OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF ADAMS, STATE OF COLORADO

DEVELOPMENT STANDARD NOTES

DESIGN STANDARDS.

- PLANNING AREA SIZES ARE SUBJECT TO MINOR MODIFICATIONS DURING FINAL PLAT AND/OR RE-PLAT. EXACT SIZE AND CONFIGURATION OF THE PLANNING AREAS WILL BE DETERMINED AT THE TIME OF FINAL PLAT AND/OR RE-PLAT.
- MINIMUM LOT WIDTH SHALL BE MEASURED FROM THE FRONT SETBACK LINES.
 THE DEVELOPMENT WILL COMPLY WITH THE CITY OF BRIGHTON RESIDENTIAL DESIGN STANDARDS WHERE THE FINAL PUD DOES NOT ADDRESS SPECIFIC.
- THE PRELIMINARY PLAT, FINAL PLAT, AND/OR PLAT AMENDMENT SHALL COMPLY WITH THE RESIDENTIAL DESIGN STANDARDS FOR MR-L, MR-M AND MR-H AS SET FORTH IN THE LAND DEVELOPMENT STANDARDS OF THIS PUD.

ROADS

PA-1, PA-2, PA-3, PA-4, PA-5, PA-6 WILL FOLLOW THE ROAD SECTIONS AND STANDARDS OF THIS PUD.

PHASING

- 1 THIS DEVELOPMENT WILL BE COMPLETED IN SEVERAL PHASES. DEVELOPMENT WILL OCCUR IN A LOGICAL AND COST EFFECTIVE MANNER BASED ON MARKET CONDITIONS AND INFRASTRUCTURE EXTENSION.
- THE PACING ORDINANCE SHALL APPLY TO DEVELOPMENT OF THE BROMLEY FARMS PUD.

OPEN SPACE AND PARKS

- THE IMPROVEMENTS PLANNED FOR THIS OPEN SPACE WILL INCLUDE DRAINAGE, DETENTION, AND LANDSCAPE IMPROVEMENTS AS SHOWN ON THE CONCEPTUAL LANDSCAPE PLAN INCLUDED HEREIN, PARKS AND OPEN SPACE DEDICATIONS SHALL BE MET ACCORDING TO REGULATIONS SET FORTH IN THE BRIGHTON RESIDENTIAL DESIGN STANDARDS.
- THE DRAINAGE CHANNEL ALONG THE SOUTH BOUNDARY OF THE PROPERTY
 SHALL BE DEDICATED TO THE CITY OF BRIGHTON AND WILL BE CONSTRUCTED
 AND MAINTAINED BY THE CITY
- THE PRIVATE OPEN SPACE AND LANDSCAPE TRACTS WILL BE MAINTAINED BY THE HOA.

LIGHTING

- 1. POLE LIGHTS SHALL BE DOWNCAST, CUT-OFF TYPES NOT TO EXCEED 18 FEET IN HEIGHT.
- 2. LIGHTS WILL BE DOWNWARD DIRECTED AND SHALL NOT CAST GLARE ON ADJACENT PROPERTIES OR RIGHTS-OF-WAY.
- A MANDATORY HOA WILL BE ESTABLISHED TO PROVIDE FOR MAINTENANCE OF ALL COMMON AREAS NOT DEDICATED TO A PUBLIC ENTITY. COMMON AREAS SHALL INCLUDE AREAS DEVOTED TO LANDSCAPING AND PRIVATE DRIVES.
- STREET LIGHTING WITHIN PUBLIC RIGHTS-OF-WAY SHALL BE PROVIDED IN ACCORDANCE WITH THE CITY OF BRIGHTON STANDARDS.

LANDSCAPING

- 1. ALL OPEN SPACE AND DETENTION TRACTS AND THE PRIVATE LANDSCAPE
 TRACTS WILL BE LANDSCAPED. CONCEPTUAL LANDSCAPE PLANS ARE PROVIDED
 HEREIN.
- THE INDIVIDUAL LOTS WILL BE LANDSCAPED AS FOLLOWS: FOR SINGLE FAMILY LOTS, THE BUILDER WILL PROVIDE GRASS AND ONE TREE IN THE FRONT YARD. THE INDIVIDUAL HOMEOWNER WILL PROVIDE LANDSCAPING AND GROUND COVER IN THE REAR YARD AREA. FRONT YARD TREES INSTALLED BY THE BUILDER WILL INCLUDE A VARIETY OF SPECIES TO PROMOTE TREE DIVERSITY. LANDSCAPING FOR THE PATIO HOMES, CLUSTER HOMES AND MULTI-FAMILY TRACT DEVELOPMENTS WILL BE INSTALLED BY THE BUILDER AND MAINTAINED BY A MANDATORY HOA OR PROPERTY MANAGEMENT ENTITY.
- THE PUBLIC OPEN SPACE AND PARKS OVER 3 ACRES SHALL BE MAINTAINED BY THE CITY OF BRIGHTON. THE OPEN SPACE AND LANDSCAPED PORTIONS OF THE RIGHTS-OF- WAY THAT ARE ADJACENT TO THE BROMLEY FARMS PROPERTY INCLUDING BROMLEY LANE, CHAMBERS ROAD, INDIAN PAINTBRUSH DRIVE, AND S. 19TH AVENUE SHALL BE MAINTAINED BY THE HOA. THE ENTRANCE FEATURES SHALL BE MAINTAINED BY THE HOA.
- 4. LANDSCAPING WILL BE PHASED WITH ADJACENT DEVELOPMENT.
- 5 DETAILED LANDSCAPING, FENCING, AND SIGNAGE PLANS SHALL BE SUBMITTED WITH THE PRELIMINARY PLAT FOR ALL PLANNING AREAS, HOA LANDSCAPE AND DETENTION/RETENTION AREAS.
- 6 HOMEOWNER MAY NOT ALTER THE FINAL LOT GRADING AND DRAINAGE OR INTERFERE WITH UTILITY EASEMENTS.

FENCING REQUIREMENTS FOR PROJECT FENCING AND WALLS SHALL FOLLOW THE BRIGHTON RESIDENTIAL DESIGN STANDARDS.

SIGNAGE

- 1. A MAXIMUM OF FOUR PERMANENT IDENTIFICATION SIGNS WILL BE INSTALLED AT ENTRANCES TO THE COMMUNITY FROM BROMLEY LANE, CHAMBERS ROAD, S. 19TH AVENUE AND INDIAN PAINTBRUSH DRIVE.
- 2. CONSTRUCTION DEVELOPMENT SIGNS: NON-ILLUMINATED CONSTRUCTION SIGNAGE WILL BE ALLOWED AT EACH ENTRANCE DURING CONSTRUCTION OR MAJOR MODIFICATION. MULTIPLE SIGNS ARE ALLOWED AND SHALL BE LOCATED A MINIMUM OF 10-FEET FROM ANY PROPERTY LINE. THE TOTAL CONSTRUCTION SIGNAGE AREA ALLOWED AT EACH ENTRANCE SHALL NOT EXCEED 100 SQUARE FEET.
- 3. ALL OTHER SIGNS WILL COMPLY WITH THE GENERAL REGULATION FOR SIGNS CONTAINED WITHIN THE ZONING REGULATIONS FOR THE CITY OF BRIGHTON.

RESIDENTIAL GARAGE PLACEMENT AND DESIGN

- GARAGE PLACEMENT AND DESIGN REQUIREMENTS SHALL FOLLOW THE BRIGHTON RESIDENTIAL DESIGN STANDARDS EXCEPT FOR THE FOLLOWING:
 - A. GARAGE DOORS ON SMALL LOTS LESS THAN 6,000 SQ. FT. SHALL NOT COMPRISE OF MORE THAN FIFTY-FIVE (55) PERCENT OF THE FRONT FAÇADE OF THE PRINCIPAL DWELLING STRUCTURE FOR TWO-CAR GARAGE

ARCHITECTURAL GENERAL DESIGN AND MATERIALS

- THE BROMLEY FARMS PUD WILL ADHERE TO THE EXISTING BRIGHTON RESIDENTIAL DESIGN STANDARDS FOR RESIDENTIAL LOT AND BUILDING DESIGN AS WELL AS MULTI-FAMILY BUILDING DESIGN.
- 2. IT IS THE INTENT IN FOLLOWING THE CITY OF BRIGHTON'S DESIGN GUIDELINES THAT BUILDERS IN THE BROMLEY FARMS COMMUNITY WILL DEVELOP SINGLE FAMILY AND MULTI-FAMILY UNITS THAT REFLECT A HIGH LEVEL OF QUALITY, DETAIL AND CRAFTSMANSHIP.
- TRADITIONAL ARCHITECTURAL STYLES SUCH AS FARMHOUSE, VICTORIAN, CRAFTSMAN, PRAIRIE, FOUR SQUARE, (AND MODERN INTERPRETATIONS OF EACH), ARE ENCOURAGED TO BLEND WITH THE CITY OF BRIGHTON'S ARCHITECTURAL HERITAGE.
 - a. UP TO 20 PERCENT OF THE COMMUNITY MAY INCLUDE A MODERN
 ARCHITECTURAL STYLE AS LONG AS IT IS GROUPED WITHIN A RESIDENTIAL
 BLOCK AND ADHERES TO THE BRIGHTON RESIDENTIAL DESIGN STANDARDS.

PLANNED UNIT DEVELOPMENT -MIXED RESIDENTIAL GUIDELINES

THE BROMLEY FARMS PUD PLAN PROVIDES FLEXIBILITY TO ACCOMMODATE SINGLE FAMILY ATTACHED AND DETACHED, AND MULTI-FAMILY LAND USES.

THE BROMLEY FARMS COMMUNITY SHALL NOT EXCEED THE TOTAL ALLOWABLE 843 RESIDENTIAL UNITS, CARRIAGE UNITS SHALL NOT COUNT AGAINST THE TOTAL ALLOWABLE RESIDENTIAL UNITS, AND ARE LIMITED TO 30% OF THE TOTAL NUMBER OF SINGLE FAMILY DETACHED UNITS.

THE LAND USES IDENTIFIED ON THE PUD PLAN ALLOW FOR FLEXIBILITY TO RESPOND TO MARKET CONDITIONS WHILE MAINTAINING THE CHARACTER AND INTEGRITY OF THE DEVELOPMENT PLAN. AT TIME OF FINAL PLAT AND/OR RE-PLAT, LAND USE AREAS IN REGARDS TO THEIR BOUNDARIES MAY BE ALTERED UP TO 10 % SUBJECT TO THE PROCEDURE FOR CITY REVIEW AND APPROVAL. FINAL PARCEL BOUNDARY LINES SHALL BE DETERMINED AT THE TIME OF FINAL PLAT AND/OR RE-PLAT.

AT THE DISCRETION OF THE COMMUNITY DEVELOPMENT DIRECTOR (THE "DIRECTOR"), A MINOR AMENDMENT (AN ALTERATION OF 10% OR LESS) TO THE SIZE, SHAPE, OR CONFIGURATION OF THE ORIGINAL PLANNING AREA (A "MINOR PLANNING AREA AMENDMENT") MAY BE ADMINISTRATIVELY REVIEWED BY THE DIRECTOR. HOWEVER, NO INCREASE SHALL BE ALLOWED DURING THIS PROCESS FOR THE MAXIMUM NUMBER OF UNITS. A REQUEST FOR A MINOR PLANNING AREA AMENDMENT MAY BE APPROVED OR DENIED, AT THE DISCRETION OF THE DIRECTOR. AN ADMINISTRATIVE DECISION BY THE DIRECTOR MAY BE APPEALED TO THE PLANNING COMMISSION WITHIN 30 DAYS OF A FINAL DECISION. WITHIN 30 DAYS OF AN ADMINISTRATIVE APPROVAL OF A MINOR PLANNING AREA AMENDMENT, THE APPLICANT FOR THE MINOR PLANNING AREA AMENDMENT SHALL PREPARE A PUD AMENDMENT DOCUMENT, ACCORDING TO THE DIRECTOR'S SPECIFICATIONS, WHICH SHALL BE RECORDED WITH THE COUNTY

PLANNING AREAS HAVE A RANGE OF DENSITIES DEPENDING ON THEIR FINAL PRODUCT AND WILL BE DETERMINED BY MARKET CONDITIONS AT THE TIME OF DEVELOPMENT. IN NO CASE SHALL UNITS EXCEED THE TOTAL ALLOWABLE APPROVED FOR THE BROMLEY FARMS PUD. MIXING OF LOT TYPES IS PERMITTED IN INDIVIDUAL PLANNING AREAS AND SHALL BE IN ACCORDANCE WITH THE PUD PLAN.

OPEN SPACE AND PARKS SHALL MEET THE REQUIREMENTS SET FORTH IN THE LAND USE AND DEVELOPMENT CODE AND THIS PUD DOCUMENT AT THE TIME OF FINAL PLAT AND/OR RE-PLAT FOR EACH DEVELOPMENT PHASE. COMPUTATIONS OF OPEN SPACE SHALL BE PROVIDED FOR THAT PORTION OF THE BROMLEY FARMS COMMUNITY. THE OPEN SPACE NETWORK AND PARKS SHALL BE DEVELOPED IN EACH PHASE CONSECUTIVE WITH HOME CONSTRUCTION. IN ADDITION, AT THE TIME OF FINAL PLAT AND/OR RE-PLAT EACH DEVELOPMENT PHASE SHALL PROVIDE A TRACKING OF TOTAL UNITS AND OPEN SPACE WITHIN THE PLAT.

OPEN SPACE TABLE

REQUIRED OPEN SPACE AND PARK AREA PER THE CURRENT BRIGHTON MUNICIPAL CODE AND BASED ON THE MINIMUM 516 UNITS:

- A. NEIGHBORHOOD PARK REQUIREMENT (3 ACRES/1000 POPULATIONS @ 2.96 PERSONS/HOUSEHOLD) OR APPROXIMATELY 4.6 ACRES.
- COMMUNITY PARK REQUIREMENT (3 ACRES/ 1,000 POPULATION @ 2.96 PERSONS/HOUSEHOLD) OR APPROXIMATELY 4.6 ACRES.
- OPEN SPACE REQUIREMENTS (15 ACRES/1000 POPULATIONS @ 2.96 PERSONS/HOUSEHOLD) OR APPROXIMATELY 22.9 ACRES
- D. TOTAL PARKS AND OPEN SPACE REQUIREMENT IS 32.1 ACRES

PROVIDED OPEN SPACE/PARKS LAND DEDICATION IS PLANNED FOR 37.7 ACRES AND INCLUDES THE AREAS OF THE FUTURE DRAINAGE OUTFALL, THE FULTON DITCH, THE PERIMETER BUFFERS, THE IDENTIFIED PARKS AND OPEN SPACE, AND THE BRIGHTON OASIS AQUATIC PARK (BROMLEY FARMS SUBDIVISION, TRACTS B & G). THIS TOTAL ACREAGE SUPPORTS UP TO 606 DWELLING UNITS WITHIN THE BROMLEY FARMS PUD. THE SUMMARY TABLE PROVIDED BELOW IDENTIFIES THE MAXIMUM NUMBER OF DWELLING UNITS SUPPORTED BASED ON THE PROVIDED PARKS AND OPEN SPACE ACREAGE. DEVELOPMENT WITHIN A PLANNING AREA THAT EXCEEDS THE MAXIMUM ALLOCATED DWELLING UNITS WILL REQUIRE A FEE-IN-LIEU PAYMENT FOR UNITS DEVELOPED IN EXCESS OF THE ALLOCATED AMOUNT.

And the second s		MAXIMUM DU'S			
		SUPPORTED			
		PRIOR TO A FEE-			
		IN-LIEU PAYMENT	ALLOCATION OF F		
PLAN	INING AREA		OPEN SPACE AREA F	,	
		(DU's)		(ACRES)	
			Neighborhood Park	1.8	
	1	198	Community Park	1.8	
			Open Space	8,8	
			Total	12.4	
			Neighborhood Park	0,9	
Controlly .	2	100	Community Park	0.9	
	•	100	Open Space	4,4	
			Total	6.2	
		t in the Control of t			949.943
			Neighborhood Park	0.8	
			Community Park	0.8	
	3	90	Open Space	4.0	
			Total	5.6	usi-asaiwaaiaeeaaea
				and a ball	
			Neighborhood Park	0.3	
			Community Park	0.3	
	4	36			
			Open Space	1.6	Anna and a grant of the state o
			Total	2.2	
	\$5551.150	4114799199551555	*** * *** ** *** ****		
			Neighborhood Park	1.4	
	5	160	Community Park	1.4	
			Open Space	7.1	
			Total	9.9	
}				<u> </u>	
			Neighborhood Park	0.2	
	6	ا الله الأوراد الله الأوراد الله الأوراد الله الله الله الله الله الله الله ال	Community Park	0.2	
			Open Space	1.0	
			Total	1.4	
TOTAL		606		37.7	7
		200 mm - 1 - 1 - 1 - 1 - 1 - 1 - 1 - 1 - 1	orrande de la companya de la company La companya de la co		

F INCLUDES 6.4 ACRES FROM TRACTS B & G OF BROMLEY FARMS SUBDIVISION FOR COMMUNITY PARK AND OPEN SPACE CREDIT TO THIS PUD.





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FINAL PUD PLAN EVELOPMENT STANDARD

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FINAL PUD PLAN

PART OF THE NORTHWEST QUARTER OF SECTION 17, TOWNSHIP 1 SOUTH, RANGE 66 WEST OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF ADAMS, STATE OF COLORADO

DEVELOPMENT STANDARDS

MIXED RESIDENTIAL - LOW (MR-L)

- 10 USES BY RIGHTS
 - A: ALL USES ALLOWED IN THE R-1, R-1-A, R-1-B ZONING CATEGORIES:
 - B. PATIO HOMES, CLUSTER HOMES, PAIRED HOMES, ZERO LOT LINE AND TRADITIONAL SINGLE FAMILY HOMES.
- GROSS DENSITY (MAXIMUM) 6.0 DWELLING UNITS PER ACRE.
- MINIMUM DIMENSIONAL STANDARDS

PLANNING AREAS PA-1, PA-2, PA-3, PA-5 (MINIMUM):

- A: LOT AREA- 4,000 SQUARE FEET.
- B. LOT WIDTH AT BUILDING LINE-INTERIOR 40 FEET
- C. LOT WIDTH AT BUILDING LINE-CORNER 50 FEET.

PLANNING AREAS PA-4, PA-6 (MINIMUM)

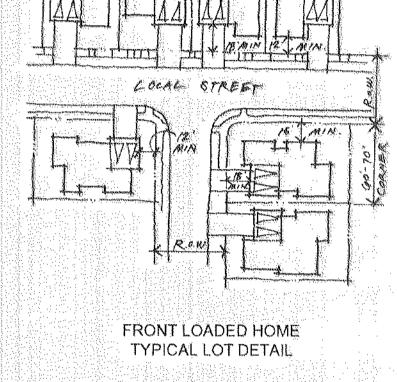
- A. LOT AREA- 5,000 SQUARE FEET.
- B. LOT WIDTH AT BUILDING LINE-INTERIOR 50 FEET
- G. LOT WIDTH AT BUILDING LINE-CORNER 60 FEET.

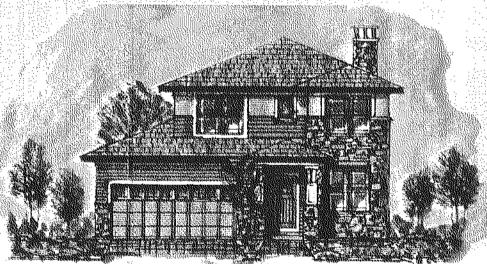
BUILDING SETBACKS (MINIMUM)

- A: FRONT (GARAGE DOOR TO BACK OF SIDEWALK/ PROPERTY LINE): 20 FEET FOR LOTS 7,000 SQUARE FEET OR GREATER 18 FEET FOR LOTS LESS THAN 7,000 SQUARE FEET.
- B. FRONT (PRINCIPAL BUILDING OR SIDE LOADED GARAGE) TO BACK OF SIDEWALK/PROPERTY LINE: 12 FEET,
- G. FRONT (PORCHES, WHICH MEET THE REQUIREMENTS OF THE RESIDENTIAL DESIGN STANDARDS) MAY EXTEND FIVE (5) FEET INTO THE REQUIRED FRONT YARD SETBACK.
- D. SIDE (INTERIOR LOT): 5 FEET
- E. SIDE (CORNER LOT): 15 FEET
- F. REAR (PRINCIPAL BUILDING): 15 FEET
- G: REAR (ACCESSORY BUILDING OR ALLEY LOADED GARAGE) 3 FEET
- H. FRONT SETBACKS SHALL BE MEASURED FROM PROPERTY LINE
- ACCESSORY BUILDING SETBACKS
- A. REAR: 3 FEET
- B. SIDE: 5 FEET
- C. MAY NOT EXTEND BEYOND FRONT ELEVATION ON FRONT OR CORNER

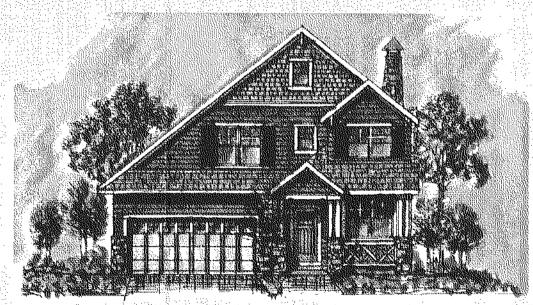
ENCROACHMENTS:

- A. ARCHITECTURAL FEATURES INCLUDING BUT NOT LIMITED TO EAVES, BAY WINDOWS, POT SHELVES, CANTILEVERS AND FIREPLACES MAY ENCROACH INTO THE REQUIRED SETBACKS PER THE CITY OF BRIGHTON CODE.
- B. UNENCLOSED AT-GRADE PATIOS OR DECKS (NO MORE THAN 12 INCHES ABOVE GRADE) MAY ENCROACH INTO ANY BUILDING SETBACK (REAR AND SIDE ONLY) TO WITHIN FIVE FEET (5') OF A PROPERTY LINE.
- C. FRONT SETBACKS SHALL BE MEASURED FROM BACK OF WALK/PROPERTY LINE:
- D. ENCROACHMENTS SHALL NOT EXTEND INTO EASEMENTS.
- BUILDING HEIGHT- PRINCIPAL BUILDING (MAXIMUM): 2 STORIES, 35
- A: COMMON AREA ACCESSORY STRUCTURES (MAXIMUM): 1 STORY, B. RECREATION BUILDINGS (MAXIMUM): 2 STORY, 30 FEET,
- PARKING- PER THE CITY OF BRIGHTON CODE.

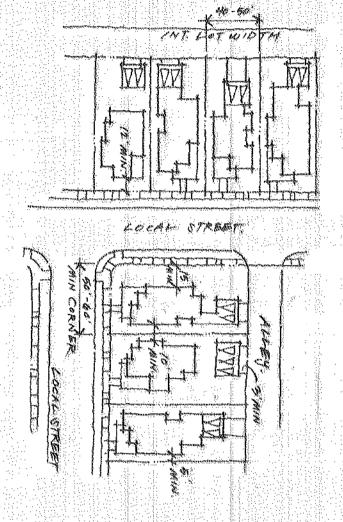




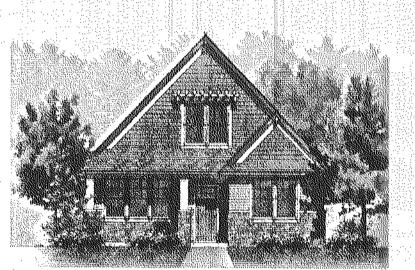
FRONT LOADED SFD ARCHITECTURAL CHARACTER ELEVATION



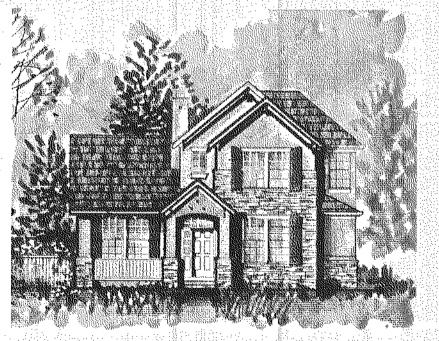
FRONT LOADED SFD ARCHITECTURAL CHARACTER ELEVATION



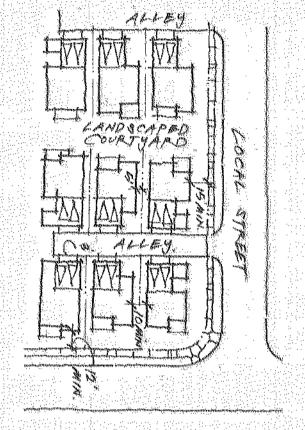
ALLEY LOADED HOME TYPICAL LOT DETAIL



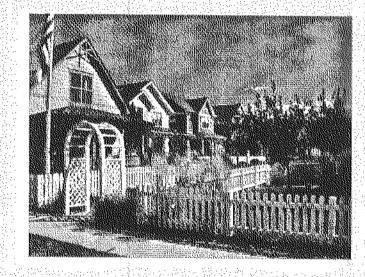
ALLEY LOADED SFD ARCHITECTURAL CHARACTER ELEVATION



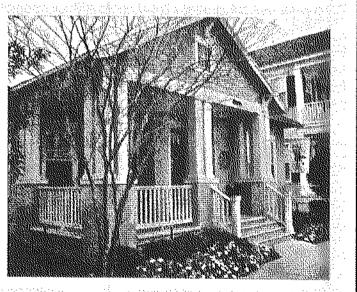
ALLEY LOADED SFD ARCHITECTURAL CHARACTER ELEVATION



CLUSTER HOME TYPICAL LOT DETAIL



CLUSTER HOME ARCHITECTURAL CHARACTER ELEVATION



CLUSTER HOME ARCHITECTURAL CHARACTER ELEVATION



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N/A	HORZ SCALE			
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		REVISE PER CITY	5	10-13-2011
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			No. of Lot, Street, or other Persons	

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FINAL PUD PLAN

PART OF THE NORTHWEST QUARTER OF SECTION 17, TOWNSHIP 1 SOUTH, RANGE 66 WEST OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF ADAMS, STATE OF COLORADO

DEVELOPMENT STANDARDS

MIXED RESIDENTIAL -MEDIUM (MR-M)

- 1 USES BY RIGHT
 - A. ALL USES ALLOWED IN THE R-1, R 1-A, R-1-B, R-2 ZONING CATEGORIES.
 - B. RESIDENTIAL- MR-L, DUPLEX, TRIPLEX, AND TOWNHOMES, AND GROUP FACILITIES.
- 2 GROSS DENSITY (MAXIMUM) 10 DWELLING UNITS PER ACRE
- 3. MINIMUM DIMENSIONAL STANDARDS: SINGLE FAMILY ATTACHED
 - A: LOT AREA- 1,800 SQUARE FEET
 - B. CORNER LOT AREA -2,500 SQUARE FEET.
 - C. LOT WIDTH AT BUILDING LINE- INTERIOR 24 FEET
 - DE LOT WIDTH AT BUILDING LINE-CORNER 34 FEET.
- 4. BUILDING SETBACKS:
 - A. FRONT (GARAGE DOOR TO BACK OF SIDEWALK, OR TO LOT LINE IF NO WALK) 18 FEET
 - B. FRONT (PRINCIPAL BUILDING OR SIDE LOADED GARAGE): 8 FEET FRONT (PORCHES) MAY EXTEND FIVE (5) FEET INTO THE REQUIRED FRONT YARD SETBACK.
 - G. SIDE (INTERIOR LOT): 5 FEET
 - D. SIDE (ATTACHED UNITS): 0 FEET
 - E. SIDE (CORNER LOT): 15 FEET
 - F. REAR (PRINCIPAL BUILDING): 15 FEET
 - E. REAR (ACCESSORY BUILDING OR ALLEY LOADED GARAGE) 3 FEET
 - F. FRONT SETBACKS SHALL BE MEASURED FROM PROPERTY LINE
- 5 ACCESSORY BUILDING SETBACKS:
 - A. REAR: 3 FEET
 - B. SIDE SFEET
- C. MAY NOT EXTEND BEYOND FRONT ELEVATION ON FRONT OR CORNER
- 6. BUILDING SEPARATION (MINIMUM):

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1-STORY	10	FEET
2-STORY	15	FEET
2.5-STORY	20	FEET
	ale:	

SIDE TO FRONT:

1-STORY	20 FEET
2-STORY	25 FEET
3-STORY	25 FEET

SIDE TO BACK

1::	1-STORY	20	FEE
- ::	2-STORY	25	FEET
	2.5-STORY	25	FEET
4.	3.3.3.3.3.1.1.1.1.1.1.1.1.1.1.1.1.1.1.1		. 994

FRONT TO FRONT

	1-STORY	25	FEE!
	2-STORY	25	FEET
:	2.5-STORY	30	FEET

BACK TO BACK

1-STORY	30 FEET
2-STORY	40 FEET
2.5 STORY	40 FEET

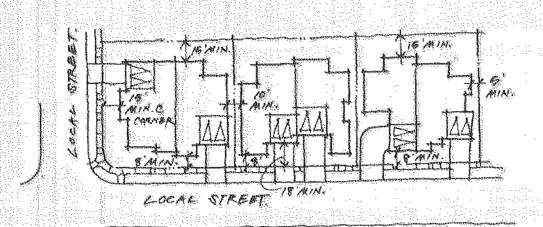
FRONT TO BACK:

50 FEET (DISCOURAGED)

GARAGE SEPARATION FROM PRIMARY STRUCTURE: 10 FEET

- BUILDING HEIGHT- PRINCIPAL BUILDING (MAXIMUM): 2 STORIES, 35 FEET.
 A. COMMON AREA ACCESSORY STRUCTURE (MAXIMUM): 1 STORY, 25
 - B. RECREATION BUILDINGS (MAXIMUM): 2 STORY, 30 FEET.
- 8 ENCROACHMENTS
 - A ARCHITECTURAL FEATURES INCLUDING BUT NOT LIMITED TO, EAVES, BAY WINDOWS, POT SHELVES, CANTILEVERS AND FIREPLACES MAY ENCROACH INTO THE REQUIRED SETBACKS PER THE CITY OF BRIGHTON CODE.
- B. UNENCLOSED AT-GRADE PATIOS OR DECKS (NO MORE THAN 12 INCHES ABOVE GRADE) MAY ENCROACH INTO ANY BUILDING SETBACK (REAR AND SIDE ONLY) TO WITHIN FIVE FEET (5') OF A PROPERTY LINE.

 G. ENCROACHMENTS MAY NOT EXTEND INTO EASEMENTS:
- distribution of the control of the c
- 9. PARKING PER THE CITY OF BRIGHTON CODE.



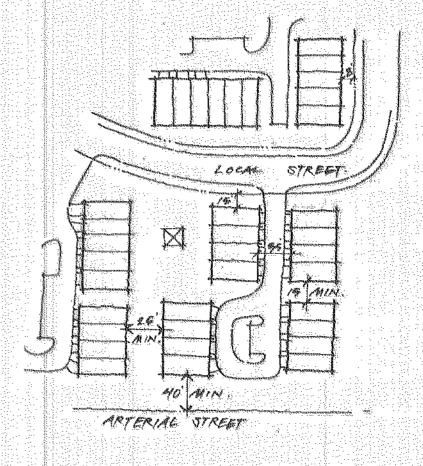
DUPLEX
TYPICAL LOT DETAIL



DUPLEX HOME ARCHITECTURAL CHARACTER ELEVATION



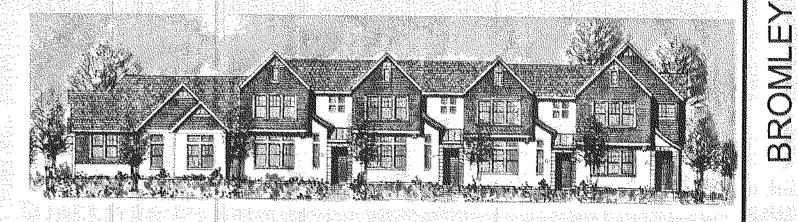
DUPLEX HOME
ARCHITECTURAL CHARACTER ELEVATION



TOWNHOME
TYPICAL LAYOUT



TOWNHOME
ARCHITECTURAL CHARACTER ELEVATION



TOWNHOME ARCHITECTURAL CHARACTER ELEVATION



Engineering Great Place

South Lincoln Street, #2008 | University Co. Street #2008 | University

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			in the
	REVISE PER CITY	5	10-13-2011
APPROVED	REVISE PER CITY	4	09-09-2011
CHECKED FG	REVISE PER CITY	3	08-16-2011
DRAWN DEN	REVISE PER CITY	2	07-14-2011
	REVISE PER CITY	¥:-	05-09-2011

BROMLEY FARMS

FVELOPMENT STANDARDS (MR-M)

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FINAL PUD PLAN

PART OF THE NORTHWEST QUARTER OF SECTION 17, TOWNSHIP 1 SOUTH, RANGE 66 WEST OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF ADAMS, STATE OF COLORADO

DEVELOPMENT STANDARDS

MIXED RESIDENTIAL - HIGH (MR-H)

- 1. USES BY RIGHT:
 - A. ALL USES ALLOWED IN THE, MR-L, MR-M AND UNDERLYING CITY OF BRIGHTON R-3 ZONING.
 - B. GROUP FACILITIES, APARTMENTS AND CONDOMINIUMS
- 2. GROSS DENSITY (MAXIMUM) 24 DWELLING UNITS PER ACRE
- BUILDING SETBACKS:
 - A. ARTERIAL STREET (ROW): 40 FEET
 - B. COLLECTOR STREET (ROW): 30 FEET
 - C. LOCAL STREET (ROW): 15 FEET
 - D. PARKING AREAS AND COMMON DRIVES: 10 FEET
 - E MINIMUM DISTANCE BETWEEN STRUCTURES- PER CITY OF BRIGHTON CODE.
 - F. BUILDING TO PARKING SETBACK SHALL BE 10 FEET MEASURED FROM BACK OF WALK
- 4. BUILDING SEPARATION (MINIMUM):

•	(1 4 04	A. 1-4-3	•	•••	٠,	٠	'	٩	
S	IDE	TO	S	IT.)E				

	1-STORY	10	FEET
	2-STORY	15	FEET
	2.5-STORY	20	FEET
100	A CONTRACT CONTRACT TO A CONTRACT OF THE CONTR		11 (11)

SIDE TO FRONT:

	1-STORY	20 FE
7	2-STORY	25 FEE
	3-STORY	25 FEE

SIDE TO BACK

1-STOR	γ				20	FEE
2-STOR	γ				25	FEE.
2.5-STO	RY				25	FEE.
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FRONT TO FRONT:

÷.	1-STORY	25	FEE
	2-STORY	25	FEE
ė	2.5-STORY	30	FEE

BACK TO BACK

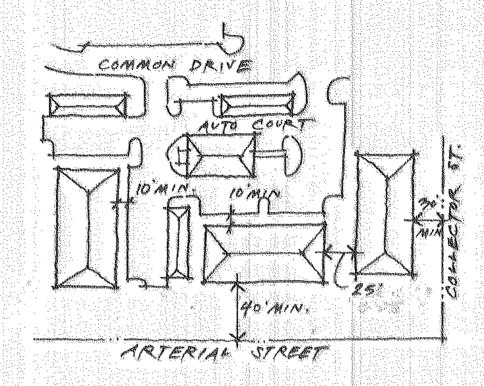
A,	1-STORY	30	FEET
	2-STORY	40	FEET
ij.	2.5 STORY	40	FEET

FRONT TO BACK:

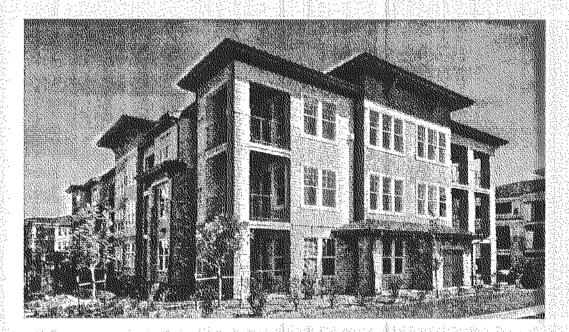
50 FEET (DISCOURAGED)

GARAGE SEPARATION FROM PRIMARY STRUCTURE: 10 FEET

- BUILDING HEIGHT (MAXIMUM): 3 STORIES OR 45 FEET
 - A COMMON AREA ACCESSORY STRUCTURE (MAXIMUM): 1 STORY, 25 FEET
 - B RECREATION BUILDINGS (MAXIMUM): 2 STORIES, 35 FEET
- 6. BUILDING AREAS (MINIMUM):
 - A ONE-BEDROOM UNITS: 500 SQUARE FEET
 - B. BEDROOM UNITS: 750 SQUARE FEET
 - C THREE BEDROOM UNITS: 900 SQUARE FEET
- 7. NUMBER OF ATTACHED UNITS PER BUILDING (MAXIMUM):
 - A TOWNHOMES
 B APARTMENTS
- 8 UNITS MAXIMUM (
- 8 ENCROACHMENTS
 - ARCHITECTURAL FEATURES INCLUDING BUT NOT LIMITED TO EAVES, BAY WINDOWS, POT SHELVES, CANTILEVERS AND FIREPLACES MAY ENCROACH INTO THE REQUIRED SETBACKS PER THE CITY OF BRIGHTON CODE.
 - B UNENCLOSED AT-GRADE PATIOS OF DECKS (NO MORE THAN 12 INCHES ABOVE GRADE) MAY ENCROACH INTO ANY BUILDING SETBACK (REAR AND SIDE ONLY) TO WITHIN FIVE FEET (5') OF A PROPERTY LINE.
 - C. ENCROACHMENTS MAY NOT EXTEND INTO EASEMENTS:
- 9. PARKING PER THE CITY OF BRIGHTON CODE



CONDOMINIUM / APARTMENT TYPICAL LAYOUT (BASED ON 3 STORY)



CONDOMINIUM / APARTMENT
ARCHITECTURAL CHARACTER ELEVATION



GARDEN APARTMENT
ARCHITECTURAL CHARACTER ELEVATION



Engineering Great Places

South South Lincoln Street, #208 | Linguistion, \$2,83,922

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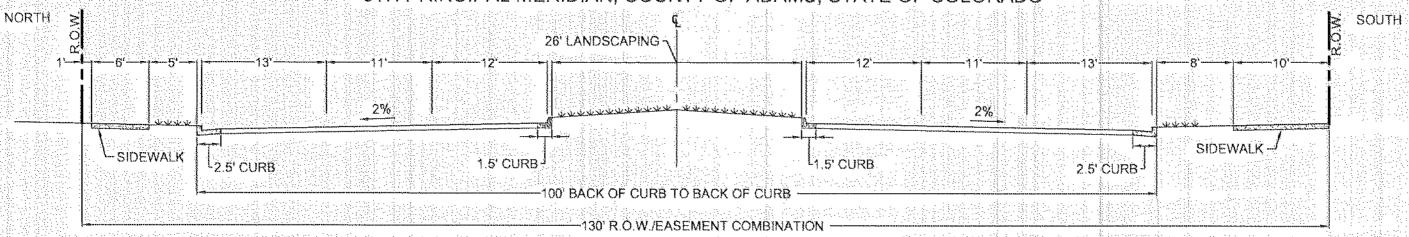
BROWLEY FARMS
FINAL PUD PLAN

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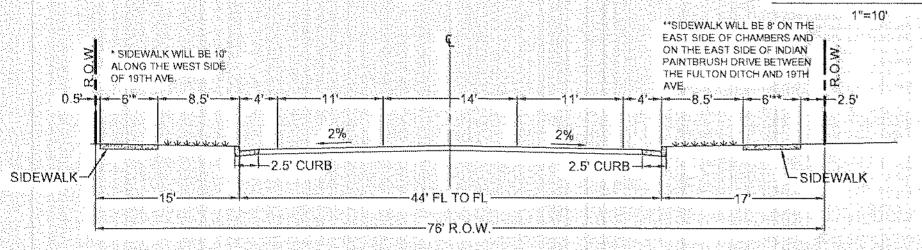
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FINAL PUD PLAN

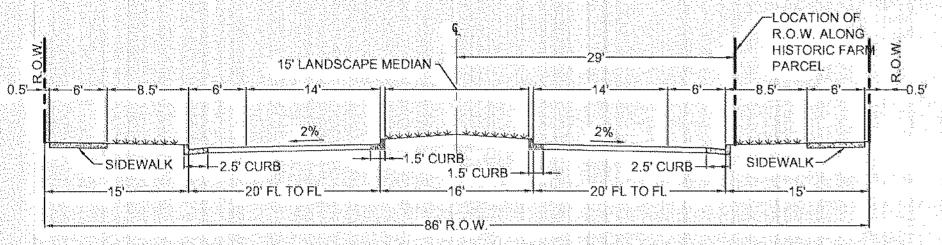
PART OF THE NORTHWEST QUARTER OF SECTION 17, TOWNSHIP 1 SOUTH, RANGE 66 WEST OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF ADAMS, STATE OF COLORADO



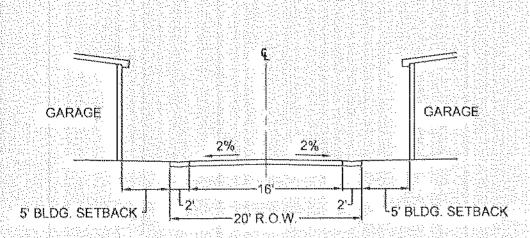
ARTERIAL - A.1



COLLECTOR - C.1, NO PARKING



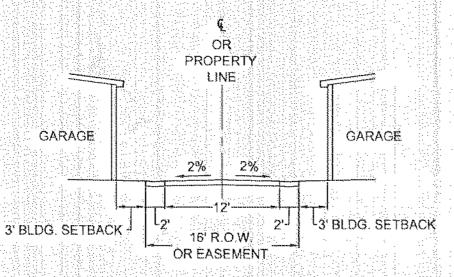
COLLECTOR - C.3, NO PARKING, WITH MEDIAN



ALLEY AL.1, NO PARKING

1. NOT ALLOWED FOR FIRE DEPARTMENT
ACCESS. ANY ALTERNATE SECTION
INTENDED TO SERVICE AS EMERGENCY
ACCESS, SHALL BE APPROVED BY THE FIRE
DEPARTMENT PRIOR TO USE.

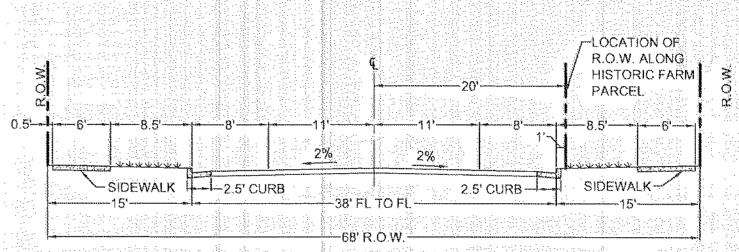
 DEPENDING ON SPECIFIC SITE CONDITIONS ALLEY'S MAY ALSO HAVE AN INVERTED CROWN WITH DRAINAGE DOWN TO THE CENTER.



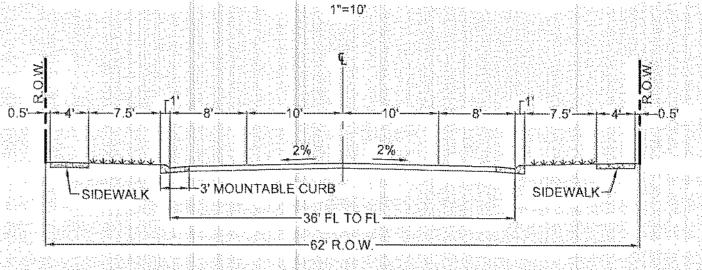
ALLEY AL.2, NO PARKING

1. NOT ALLOWED FOR FIRE DEPARTMENT
ACCESS. ANY ALTERNATE SECTION
INTENDED TO SERVICE AS EMERGENCY
ACCESS, SHALL BE APPROVED BY THE FIRE
DEPARTMENT PRIOR TO USE.

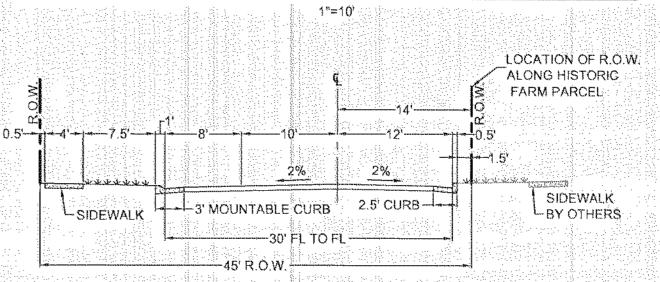
2. DEPENDING ON SPECIFIC SITE CONDITIONS, ALLEY'S MAY ALSO HAVE AN INVERTED CROWN WITH DRAINAGE DOWN TO THE CENTER.



COLLECTOR - C.2, PARKING ON BOTH SIDES



RESIDENTIAL - R.1, PARKING ON BOTH SIDES



RESIDENTIAL - R.2, PARKING ON ONE SIDE

NOTES:

- 1. SECTION A.1 IS INTENDED FOR BROMLEY LANE
- SECTION C.1 IS INTENDED FOR CHAMBERS ROAD, INDIAN PAINTBRUSH DRIVE, AND SOUTH 19TH AVENUE.
- 3. SECTION C.2 IS INTENDED FOR SOUTH 15TH AVENUE AND INDIAN PAINTBRUSH DRIVE WHERE RESIDENTIAL LOTS FRONT ON COLLECTORS.
- 4. SECTION C.3 IS INTENDED FOR SITE ENTRY LOCATED AT BROMLEY LANE AND CHAMBERS ROAD.
- ALLEYS SHALL BE OWNED AND MAINTAINED BY THE HOME OWNER'S ASSOCIATION FOR BROMELY FARMS.



	02-11-2011	OEN	FGT	T.C.I		10005	. 1*= 10	N/A	
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FINAL PUD PLAN

PART OF THE NORTHWEST QUARTER OF SECTION 17, TOWNSHIP 1 SOUTH, RANGE 66 WEST OF THE

6TH PRINCIPAL MERIDIAN, COUNTY OF ADAMS, STATE OF COLORADO **Bromley Lane** Prilimeny ahionia indules. Raldin Community S. 19th Avenue

Chambers Road

Residential Summary 1 60 Lots Front Load Homes 2 55' Lots - Front Load Homes 50 Lots - Front Load Homes 4 50' Lors - Alley Load Homes 5 45 Lots - Alley Load Homes 6 45' Lors - Alley Court Homes 7 Patited Homes a Townhomes 9 Pagaragak

148th

Avenue

1. THE ILLUSTRATIVE VISION PLAN SHALL IN NO

2. FINAL ROAD CONFIGURATION ALONG THE PA-5 NORTHERN BOUNDARY SHALL BE DETERMINED AT TIME OF FINAL PLAT AND/OR ILLUSTRATIVE IN NATURE AND IS NOT INTENDED TO REPRESENT THE FINAL ROAD LOCATION

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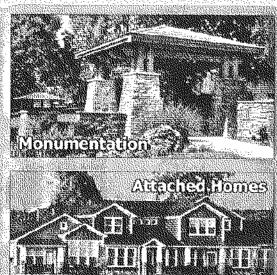
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		REVISE PER CITY	5	10-13-2011
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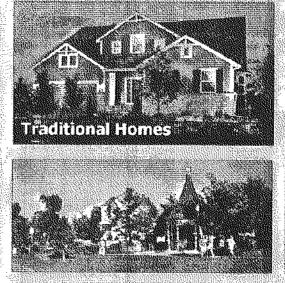
FINAL PUD PLAN

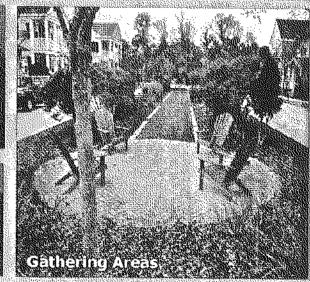
PART OF THE NORTHWEST QUARTER OF SECTION 17, TOWNSHIP 1 SOUTH, RANGE 66 WEST OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF ADAMS, STATE OF COLORADO

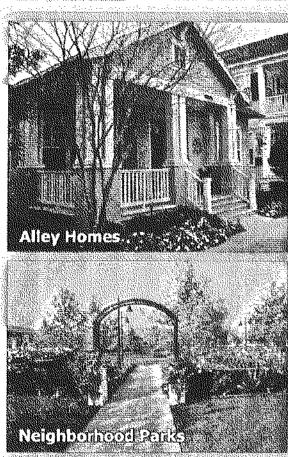


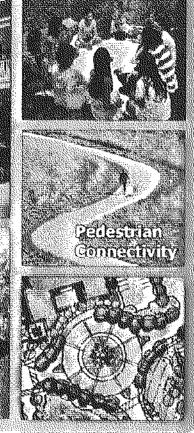


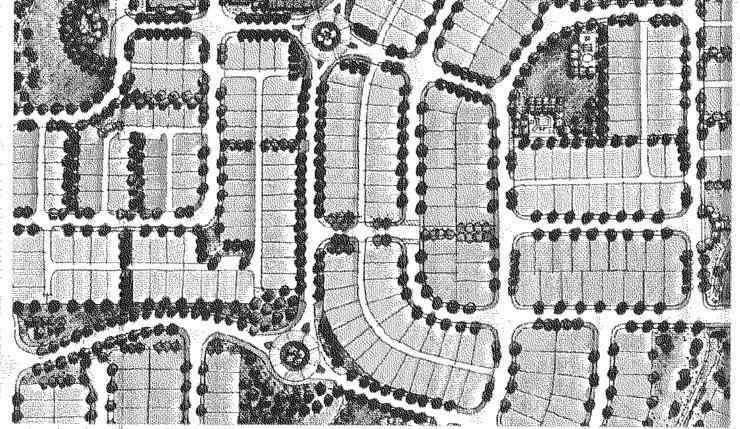


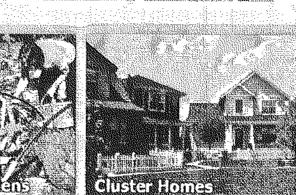








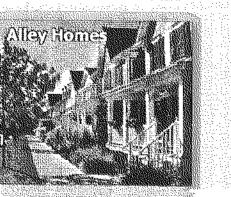




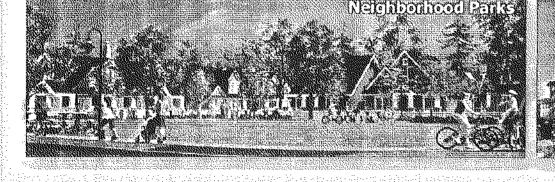




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AD MA 3 0 0 RESIDENTIAL COMMUNITY

FINAL PUD PLAN

PART OF THE NORTHWEST QUARTER OF SECTION 17, TOWNSHIP 1 SOUTH, RANGE 66 WEST OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF ADAMS, STATE OF COLORADO

Bromley Lane

Campers Again

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LEGEND:

PRIMARY PEDESTRIAN CONNECTIVITY

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NOTE:

148th

Avenue

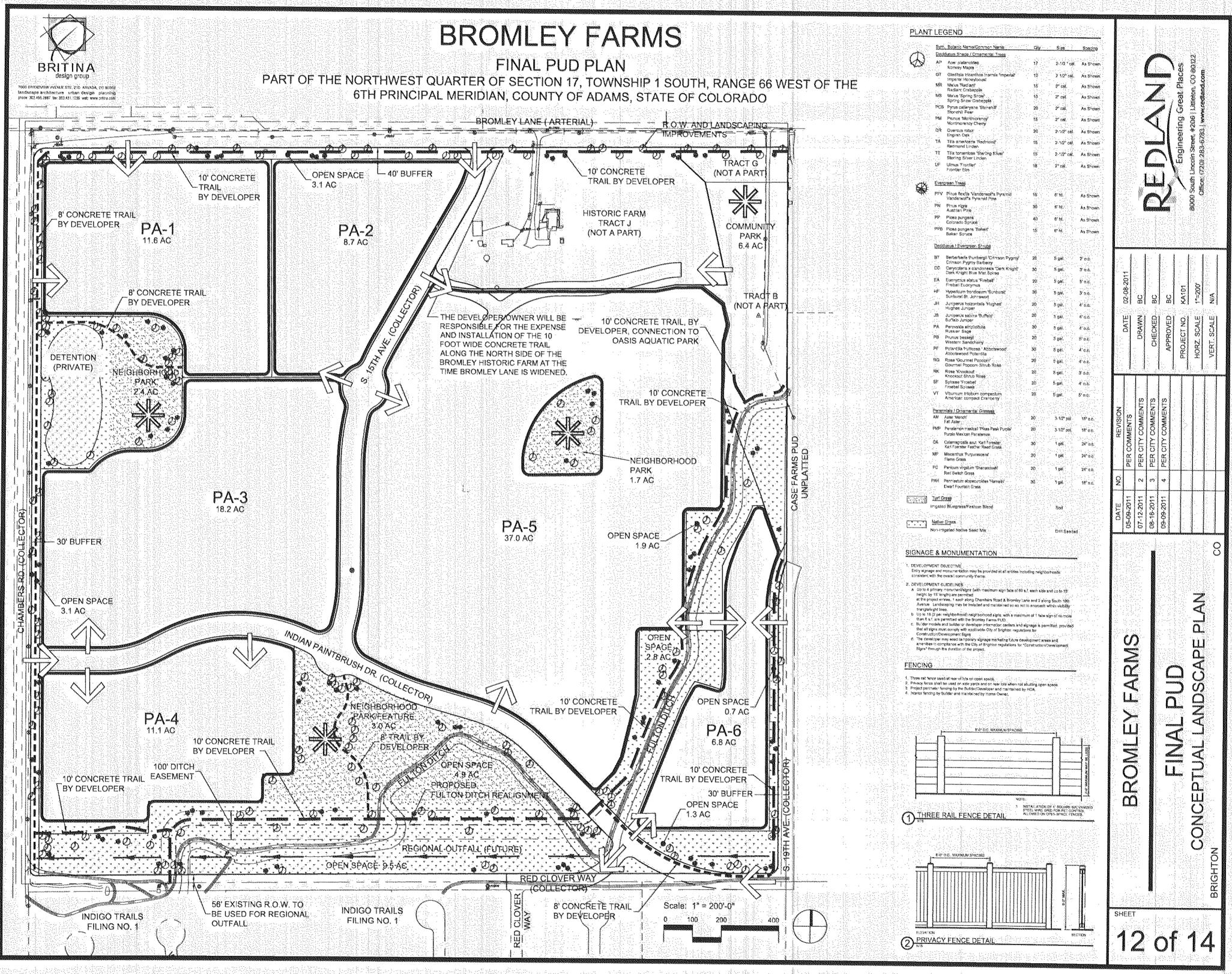
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FINAL ROAD CONFIGURATION ALONG THE PA-5

PRIMARY PEDESTRIAN CONNECTIVITY BY OTHERS

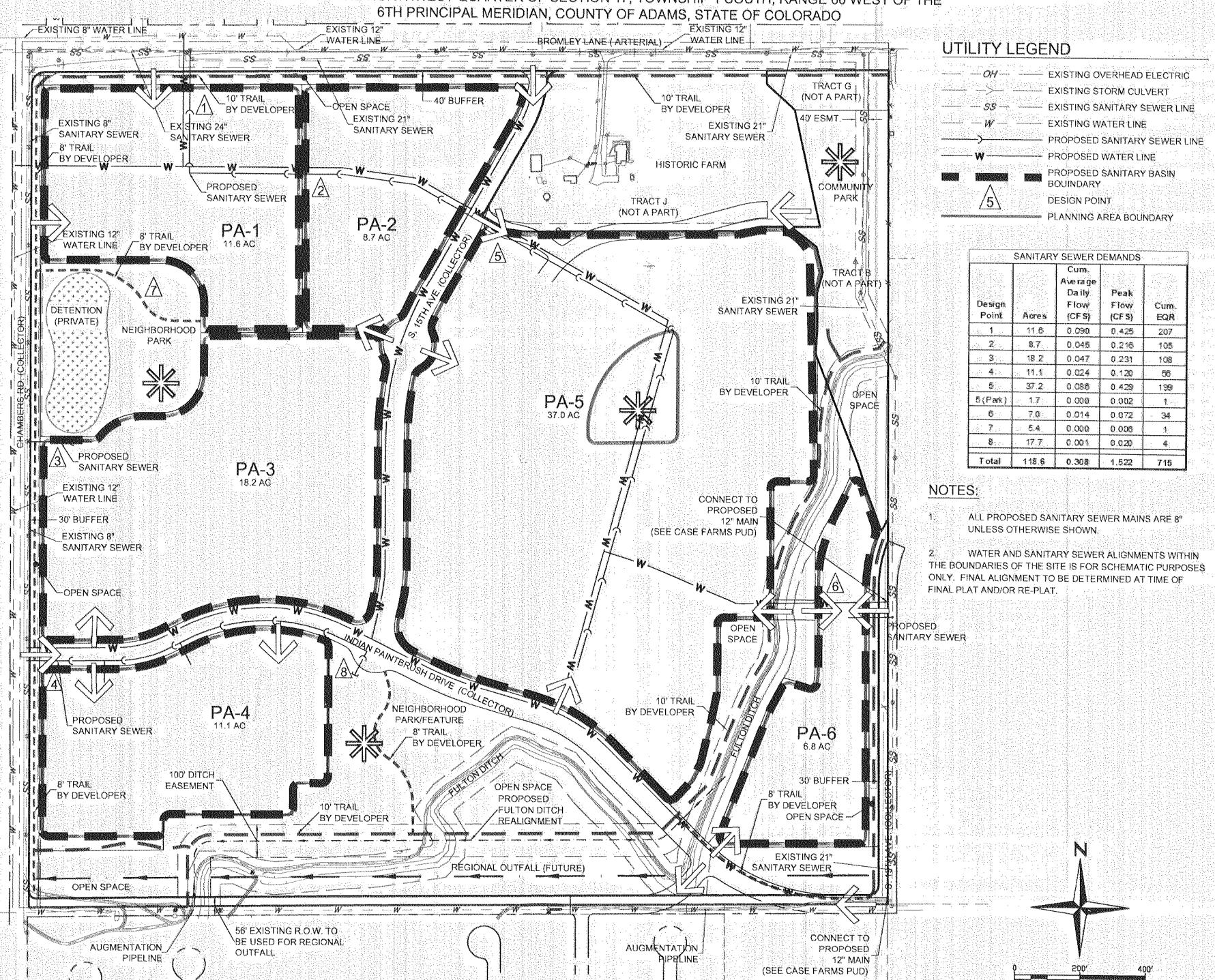
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FINAL PUD PLAN

PART OF THE NORTHWEST QUARTER OF SECTION 17, TOWNSHIP 1 SOUTH, RANGE 66 WEST OF THE





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FINAL PUD PLAN
CONCEPTUAL UTILITY PLAN

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